



# Lodgement Submission for Layout (Development Control) Plans

For  
*Landed Residential  
& Farm Developments*



## WHAT IS LTA'S LODGEMENT SCHEME?

LTA's lodgement scheme is a self-declaration scheme wherein proposed development works are checked and declared by a Qualified Person (QP) to be in full compliance with LTA's prevailing code of practice, standards and guidelines. Upon successful submission, the QP will receive an acknowledgement of lodgement from LTA.

This quick guide focuses on the lodgement of Layout Plan submissions, for developments not located within the Railway Protection Zone.

Should the development proposal fall within the Railway Protection Zone, a **separate submission** shall be made to LTA for clearance under the Rapid Transit Systems Regulations,

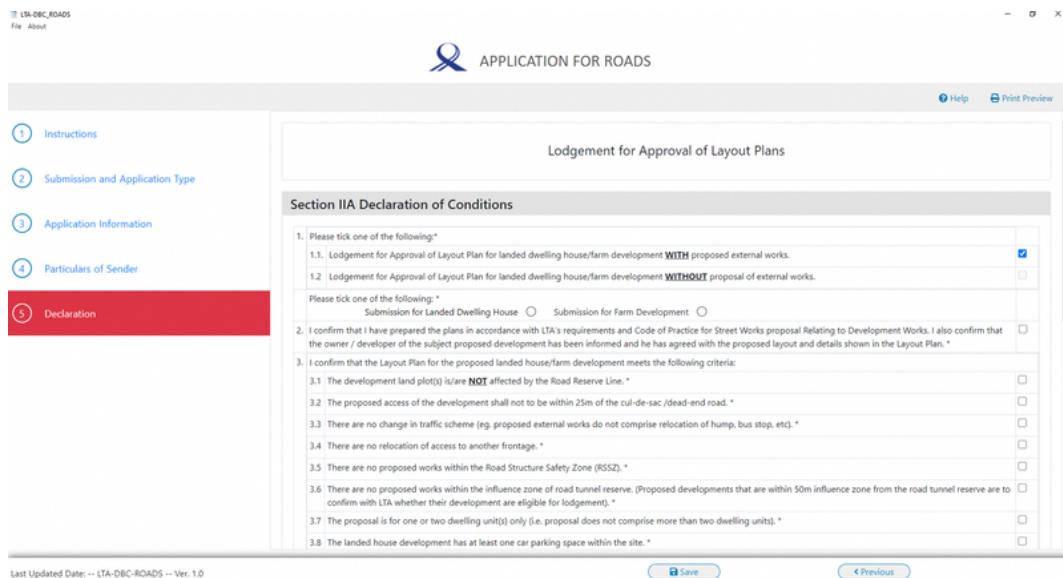
## WHAT PROPOSALS CAN BE LODGED WITH LTA AT THE LAYOUT PLAN (DEVELOPMENT CONTROL) STAGE?

Currently, Layout (Development Control) Plans for **Landed Residential** and **Farm** developments qualify for LTA's lodgement scheme. Additionally, the proposal should satisfy the following criteria:

- The proposed development plot(s) is/are not affected by Road Reserve Lines.
- The vehicular access is not relocated to another frontage.
- There are no proposed roads to be constructed.
- There are no changes to the traffic scheme (e.g. proposed external works do not include relocation of humps, bus stop, road marking, traffic signs etc.).
- There are no existing/new encroachments of the road reserve.
- There are no works carried out within the road structure safety zone.
- There are no works that are carried out within 50m from the line of tunnel reserve.
- For landed residential developments: the proposal does not comprise more than 2 dwelling units and each landed dwelling house has its own parking lot.

# HOW TO MAKE A LODGEMENT SUBMISSION TO LTA

- The QP shall lodge the Layout Plans via CORENET ESS.
- The e-Form 'LTA-DBC\_Roads.xfdx' consists of 5 sections. Please carry out the following steps:
  - Read the '*Instructions*'.
  - Under '*Submission Type*', select 'New Submission', indicate 'Application Type 1' under 'Application Type', and further indicate
    - 'YES' - If there are proposed external works within the road reserve.
    - 'NO' - If there are no proposed external works within the road reserve.
  - Under '*Application Information*', fill in the details of your application. Please ensure that your development is either a landed residential or farm development.
  - Fill in your particulars under '*Particulars of Sender*'.
  - Declare all mandatory conditions under '*Declaration*'. Please ensure that your proposal adheres to the criteria stipulated above, as well as the other conditions within the declaration section.



The screenshot displays the 'APPLICATION FOR ROADS' web interface. The left sidebar shows five sections: 1. Instructions, 2. Submission and Application Type, 3. Application Information, 4. Particulars of Sender, and 5. Declaration (highlighted in red). The main content area is titled 'Lodgement for Approval of Layout Plans' and contains 'Section IIA Declaration of Conditions'. It includes a list of conditions with checkboxes for selection. Condition 1.1 is checked. At the bottom, there are 'Save' and 'Previous' buttons, and a footer indicating 'Last Updated Date: -- LTA-DBC-ROADS -- Ver. 1.0'.

## DOCUMENTS TO SUBMIT

In your Layout Plan lodgement submission to LTA, you will need to furnish the following documents:

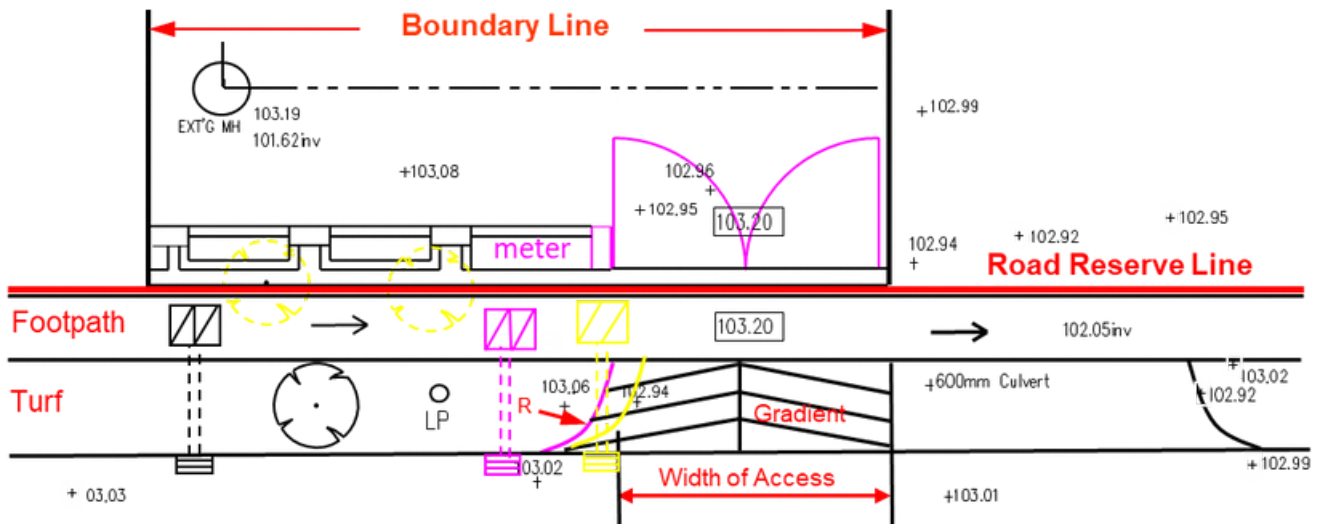
- Completed application form "LTA\_DBC\_Road.xfdx"
- One set of layout plans including:
  - Site plan
  - Floor plans
  - Section plans (including road features)
  - Elevation plans
  - Topographical plan
- Latest site photos (with date stamp) showing the existing frontage of landed dwelling house / farm development. Date of photos taken shall not be older than 1 month from date of submission.

Please ensure that all of the above documents are included in your submission.

# DETAILS TO SHOW ON THE SITE PLAN

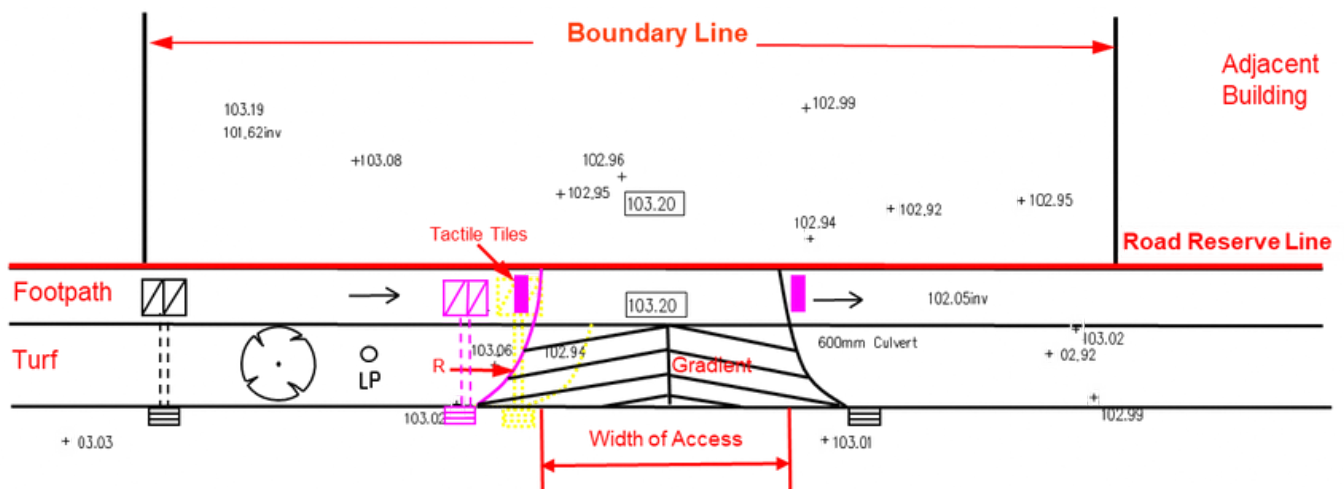
It is important to clearly indicate the proposed street works that will be carried out in your submission. Please ensure that the following details are clearly presented in your site plan:

## FOR LANDED RESIDENTIAL DEVELOPMENTS



- Show road features to be deleted in dotted yellow lines, proposed road features in magenta, existing in cyan.
- Indicate road reserve line in bold red.
- Indicate width of proposed access and footpath.
- Indicate proposed turning kerb radius.
- Indicate gradient of proposed entrance approach.
- Indicate existing road levels, proposed culvert & internal platform levels.
- Proposed top level of culvert to be higher than road edge level.
- Show location of meter compartment.
- Show proposed location of lamp post affected by proposal, if any.
- Show proposed drop inlet chamber at tangent points of turning radius.
- Show position of pedestrian side gate, if any.
- Annotate on plan whether the existing culvert is to be retained or any other proposed works within the road reserve.

## FOR FARM DEVELOPMENTS



- Show road features to be deleted in dotted yellow lines, proposed road features in magenta, existing in cyan.
- Indicate road reserve line in bold red.
- Indicate width of proposed access and footpath.
- Indicate proposed turning kurb radius.
- Indicate gradient of proposed entrance approach.
- Indicate existing road levels, proposed culvert & internal platform levels.
- Proposed top level of culvert to be higher than road edge level.
- Show proposed location of lamp post affected by proposal, if any.
- Show proposed 600mm wide yellow homogenous tactile tiles at 300mm from the kerb line on both sides of the entrance on footpath. (please refer to quick guide on "Designing Tactile Indicators for Safe Travel").
- Show proposed drop inlet chamber at tangent points of turning radius.
- Show position of pedestrian side gate, if any.
- Annotate on plan whether the existing culvert is to be retained or any other proposed works within the road reserve.



# COMMON MISTAKES

Over the years, LTA has noted several common errors made by industry professionals when lodging layout plans. They include the following:

1. **Turning kerbs which are not in line with the gate post.**
2. **Improper or non-provision of Type 'B' aluminium alloy railing.**
3. **Concrete finishings on top of culvert.**
4. **Improper levelling of culvert and access resulting in water stagnation issues.**



## COMMON MISTAKES (CONTINUED)

5. Encroachments of the road reserve.
6. Sliding gates between the boundary wall and the road reserve, posing as tripping hazard.
7. Improper or non-provision of drop inlet chambers.
8. Top-of-culvert levels sloping towards the road carriageway.



No encroachment within road reserve



Sliding gate shall be placed behind the boundary wall to prevent tripping hazard



Drop in-let chamber shall be provided at the tangent point of the turning kerb to ensure surface water runoff to be discharged at lowest point, to prevent water stagnation.



Top of culvert shall be flat to ensure safe path for pedestrians to cross entrances





# ABOUT THIS SERIES

"Quick Guides for Development Proposals is a series aiming to broaden and consolidate understanding of LTA's building plan regulations and processes. The guides within the series feature in-depth explanation of the principles behind specific requirements, coupled with examples of good practices & common mistakes.

Topics for each guide are carefully curated based on LTA's observations of prevailing trends. All publications are made available at LTA's corporate website.

### INCLUSIVE STREETS: DESIGNING TACTILE INDICATORS FOR SAFE TRAVEL

**Objectives of tactile indicators**

Tactile indicators are required where footpaths connect with driveway of developments and all-glass pedestrian crossings to guide the visually impaired pedestrians. The correct placement of tactile indicators for visual situations gives the user the safe route for the visually impaired pedestrians. Community resource placement of tactile indicators can lead to hazardous situations. This quick guide helps Architects, Engineers and Builders to identify the correct placement of tactile indicators for various situations. There are also illustrations of common errors to be avoided.

Tactile indicators are required where footpaths connect with driveway of developments and all-glass pedestrian crossings to guide the visually impaired pedestrians to cross these locations safely. Tactile indicators act as landmarks providing visual information to the user and by application must be targeted to the user's line of sight. The application of tactile indicators will vary with the user and the complexity of the situation. This quick guide helps Architects, Engineers and Builders to identify the correct placement of tactile indicators for various situations. There are also illustrations of common errors to be avoided.

The information on tactile indicators within buildings, please refer to BCA's Code on Access to Accessibility.

**Positioning tactile indicators correctly**

Follow these steps to correctly position the tactile indicators:

- Determine whether the approach is perpendicular to the driveway or on a straight, angled or curved portion of the driveway.
- To help in the placement of the tactile indicators, you may draw a imaginary line parallel to 300 mm from the edge of the driveway.
- Place 7 mm of tactile indicators touching the imaginary line as shown in Figure 2.
- Ensure that the width of the tactile indicators covers the entire width of the cross-section.

Figure 2 - Skewed  
Figure 3 - Corner

Quick Guide Series for Development Proposal, brought to you by Development & Building Control Division.

### INCLUSIVE STREETS: DESIGNING COVERED LINKWAYS FOR SHELTERED CONNECTIVITY

**1.0 Objectives of Covered Linkways**

The use of covered linkway plays an important role in achieving sheltered connectivity for pedestrians and cyclists. Properly designed covered linkways ensure weatherproof pedestrian connectivity between developments to transport users like bus stops and train stations. Community, sustainable design of covered linkway can lead to important solutions for the public.

This quick guide helps Architects, Engineers and Builders to identify the critical design elements for various types of covered linkways including both low and high covered linkways and the standards, better appreciate the principles behind the requirements and avoid common mistakes found.

**2.0 Low Covered Linkway**

**2.1 Typical Plan Presentation for a Low Covered Linkway**

Longitudinal Section, Cross Section, Roof Plan, Completed Covered Linkway

Figure 1 - Typical Plan Presentation for Low Covered Linkway Plan

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### Access Arrangements for Residential Developments

**Objectives**

With any development, the design, position and arrangement of access points are critical. They ensure that vehicles can enter or leave the development safely and do not adversely affect traffic along the public street to which it connects.

This quick guide aims to help Architects, Engineers and Builders to identify the critical design elements for the access points of residential developments, better appreciate the principles behind these requirements, and avoid making common mistakes.

**About this series**

Since April 2020, LTA has been publishing a series of quick guides to improve the industry's understanding of LTA's building plan regulations and processes. The guides feature an in-depth explanation of the principles behind specific requirements, coupled with examples of good practices & common mistakes.

Our focus will continue based on other dimensions of providing sheltered connectivity for pedestrians and cyclists.

1. Vehicular access points are located at an access point. The following access points shall be located:

- Access points shall be located at the end of the driveway.
- Where a development's access point is shared with a neighbouring development, the access point shall be located at the end of the driveway.
- Access points shall be located at the end of the driveway.
- Access points shall be located at the end of the driveway.

**2.1 Critical Design Elements for Low Covered Linkways**

The key design criteria for low covered linkways are as follows:

- Roof gradient
- Roof gradient
- Roof gradient
- Roof gradient

Figure 2 - Typical representation for Design Criteria for Low Covered Linkway

Figure 3 - Common Mistake: The design of low and high covered linkways and avoid common mistakes.

Figure 1 - Typical representation summarizing the key design criteria for low covered linkways.

### INCLUSIVE DEVELOPMENTS: DESIGNING DELIVERY SPACES WITHIN MIXED DEVELOPMENTS

**1.0 Objectives of Residential Delivery Spaces in Mixed Developments**

There is an increasing trend of mixed or integrated developments incorporating a residential component. Partly accelerated by the COVID-19 pandemic, there is also a higher demand for food, grocery and furniture deliveries to end-consumers. Designers have to adapt and cater to these trends. Designing for delivery spaces in a pure residential development is straightforward. However, designing for delivery spaces in a mixed development is more challenging.

**2.0 Design Criteria for Residential Delivery Spaces in Mixed Developments**

The key design criteria for delivery spaces (i.e. LUS bays) for residential component in all new mixed developments are as follows:

LTA Design Criteria for Residential Component	Criteria to meet
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**2.1 Critical Design Elements to follow**

The key design criteria for delivery spaces (i.e. LUS bays) for residential component in all new mixed developments are as follows:

**QUICK GUIDE SERIES FOR DEVELOPMENT PROPOSALS**

**DESIGNING SAFE DROP-OFF POINTS IN DEVELOPMENTS**

NOVEMBER 2021, ISSUE 7

**OBJECTIVES**

With in a development proposal, the design, position and arrangement of the drop-off points are critical. They ensure that sufficient space and sight distance are created for vehicles to manoeuvre safely in and out of the development.

This quick guide aims to help Architects, Engineers and Builders to identify the critical design elements for the drop-off points of developments, better appreciate the principles behind these requirements, and avoid making common mistakes.

Land Transport Authority  
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### QUICK GUIDE SERIES FOR DEVELOPMENT ISSUE 6

## Design Requirements for Integrated Development

Brought to you by Infrastructure Protection Division Development & Building Control Sub-Group In collaboration with:

**1. PREFACE**

This quick guide clarifies LTA's design requirements for developments integrated with Rapid Transit Systems (RTS). Proposed developments integrated with existing RTS may be initiated by the Developer to enhance the connectivity of the precincts. However, it is subjected to the review and approval by the Authority. It may be stipulated in Urban Redevelopment Authority (URA)'s Government Land Sales (GLS) agreements for developer to provide direct access from the proposed development to RTS stations.

The case studies presented in this series aim to explain and guide you on the basic design requirements to be incorporated in your proposal:

- UPL connection at station concourse level via station knock-out panels
- EPL connection to elevated station
- At-grade connection to station entrance via covered linkways
- General Mechanical & Electrical (M&E) provision at the interface

Figure 1: Depiction of a typical road layout for a divided 2 way road.

### Managing Manholes Displaced by Road Widening/Improvements

1st Qtr 2021

**Introduction**

Under the standard road typology (refer to Figure 2), side-table space is relevant services and utilities. As this available space is limited, it is a challenge for road widening/ improvements.

LTA allows the use of carriageway space within the public streets to house services need to comply with technical specifications, and must not affect the usability of the carriageway.

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